


AMERICA'S BOATING CLUB

For Boaters, By Boaters®




THE DRUM

A Publication of the Finger Lakes Chapter

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Report any errors or omissions to:
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From The Commander

By *Kris West, S* *SV Zephyr*

Water, Water, Everywhere...

As of this writing, Seneca Lake water level at the Geneva station measured 447.64 feet above sea level, less than half a foot below minor flood stage, resulting in a delayed spring launch. It seems unreal, but only last fall the lake was so low that some of us, myself included, fretted about whether we'd be able to reach the haul out slip without running aground. As the water levels fluctuate, I've heard questions from some of our members about what controls the level of Seneca Lake so I thought I'd take an opportunity to delve into it myself. Since we won't be out on the water until the levels go down, we might as well spend some time better understanding the hydrology of the lake. Here's what I've learned in a morning of browsing the internet.



Water levels are determined by a complex interplay between seasonal weather patterns, geology, land form (ie hills and valleys), soils, land cover, geography and human activity. To simplify matters, imagine a big



Figure 1: Water lapping at Village Marina Docks April 26, 2026.



Figure 2: Map of Seneca Lake Watershed Land Cover from Finger Lakes Land Trust.

bathtub representing the lake’s entire watershed and then imagine how water enters and leaves our ‘tub’. In the case of Seneca, the bathtub (or watershed) covers about 536 square miles (excluding the Keuka Lake subbasin which drains to Seneca Lake). Seneca Lake itself occupies an area of only 68 square miles in a narrow, deep trough gouged out by

glaciers most recently about 10,000 years ago. Notwithstanding the two major streams flowing into the lake (Keuka Outlet, near Dresden, and Catherine Creek, near Watkins Glen), most of the lake’s water comes directly from rainfall via smaller streams around the lake. If soils in the surrounding area are saturated, as has been the case

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much of this spring, most precipitation falling within the watershed will flow directly to the lake. This means that a rainfall event of only an inch or two across the watershed can result in billions of gallons of input to the lake. As the soil dries out and plants begin to grow, more rainfall can be absorbed by the surrounding watershed. During the summer season, levels typically range between 444.0 and 445.0 feet above sea level so our spring levels are more than 2 feet above what we'll expect in a few months.

Once the water gets into the lake, there are only a few ways for it to get out. Direct evaporation is one mechanism but that isn't much of a factor in cooler spring months. The main 'drain' is located near Geneva State Park where the lake enters the Seneca/Cayuga Canal which leads to Cayuga Lake. Then the water makes its way to the Atlantic by way of the combined Seneca River/Erie Canal out of Cayuga Lake and the Oswego River/Canal into Lake Ontario and finally the Saint Lawrence River.

The water level in the Seneca/Cayuga Canal can be managed at two dams but is constrained by water levels in Seneca Lake as well as water levels downstream. If downstream communities are already experiencing high water, there is limited ability to increase flow through the dams. Efforts are made to control the lake level within certain parameters at the dams, but due to variations in weather patterns, it's an inexact science at best. So, while a knee

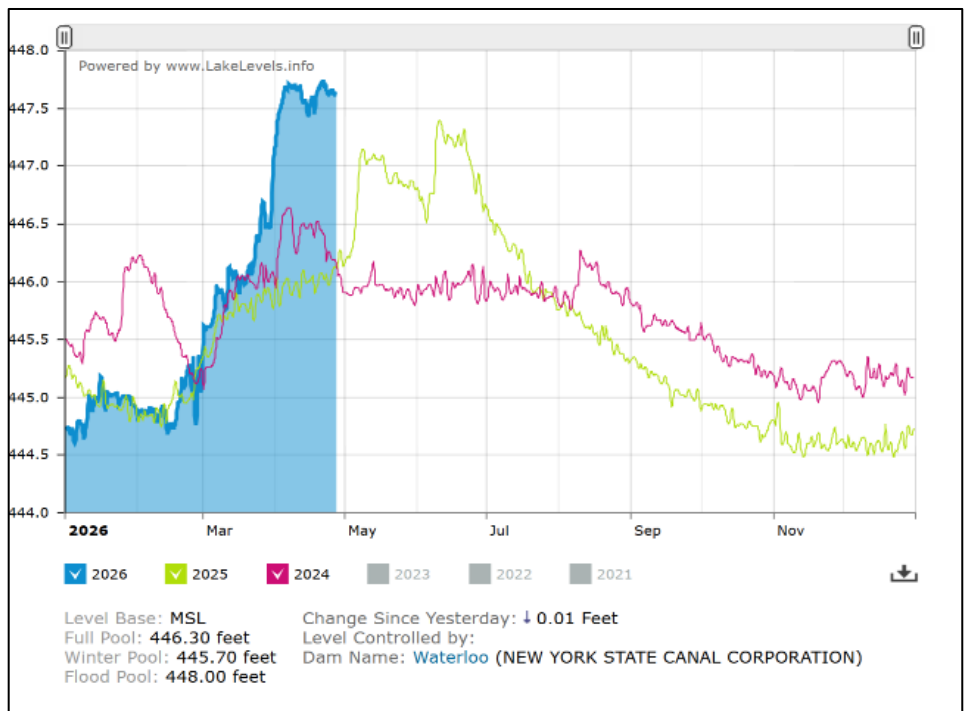


Figure 3: Seneca Lake Levels for 2024- 2026. Source: Seneca Lake Pure Waters.

jerk reaction might be to say, “Open those dam gates and let the water out!”, that won't work when our downstream neighbors have too much water themselves. Furthermore, management of the dams on the Seneca/Cayuga Canal is carried out within the larger context of water level management in the entire Oswego River Basin and that's more complicated than I can get into here. Suffice it to say conversations are being had and if you want to learn more, reach out to the Seneca Lake Pure Waters Association.

What I've learned from my morning's research is that weather patterns and land use are the two biggest factors controlling lake levels in Seneca Lake. As we all know, 'weather' is what is happening now whereas 'climate' is what you'd expect to happen in any given season. I operate with the understanding that the climate is changing and in years to come

our weather will be characterized, in part, by more frequent and intense extreme events like heavy rainstorms, seasonal droughts and heat waves. That means that we may see more “higher highs” and “lower lows” in Seneca Lake's water levels. This expectation, in turn, leads me to support local efforts to conserve natural resources, plan for responsible land use and implement smart climate policies all of which will help secure the health of the landscape, the lake and our local communities. If you're looking for local charities who are working in this space, I suggest you look up The Finger Lakes Land Trust and Seneca Lake Pure Waters Association.

Now, even if the lake is high, you can still get outside and have fun!

- Kris
co@abc-flx.org

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From the Executive Officer

By Jim McGinnis, SN-CN

SV Brewster

Terra Ceia Regatta



Ahoy all!
Lynne and I had a nice weekend on the water on the Florida Gulf Coast on April 18th at the Terra Ceia Regatta. We

rented a Catalina 30, *S/V Latitudes*, and stayed at a friend's dock on Friday and Saturday night. A couple dolphins were under the boat and swam right next to us as we entered the bay. We also saw a sad situation where a 40-foot sailboat lost power and was pulled under the Cortez Road Drawbridge by the current. The whole rig was down in the water, and the captain was being assisted by TowBoatUS. Lesson: Be ready to drop an anchor at any time.



Figure 4: Terra Ceia Anchorage.



Figure 5: Rafting off *S/V Aurora Morgan 34*.



Figure 6: Sunrise.

2026 Boater's Weekend

Please mark your calendars for the Finger Lakes Summer Boat Trip July 31st to August 2nd. We have the Pavilion at Lodi State Park reserved for Friday Night.

[Lodi Point State Park - NYS Parks, Recreation & Historic Preservation](#)

General Information [\(315\) 585-6392](tel:3155856392), Lower Lake Road, Lodi, NY 14860

Boats can anchor and raft up in the north side anchorage area as they arrive for the afternoon and then go ashore for a pitch-in dinner at the Pavilion at 6PM. There are transient slips available near the boat ramp on the south side for shallow draft boats. For those arriving by land yachts, admission to the Lodi Point State Marine Park is just \$7 from 8 AM to 6 PM. The park is open from dawn to dusk.

Saturday, we'll have an Anchoring/Mooring/Rafting Seminar as the ABC-FLX Education event starting at 9AM in the morning. Afterwards we'll head across the lake to Sapalta Restaurant at Plum Point for Brunch at 11AM. They are open 10AM to noon. Land Yachts can join us there also.

Saturday afternoon we'll set sail or motor to Miles Winery for a wine tasting on the dock. Depending on weather, boats could anchor there, at Long Point, or back at Lodi on Saturday night and head home on Sunday morning.

If you are considering attending, Please RSVP to Jim McGinnis by May 15th, 2026 with your boat



Figure 7: North side anchorage at Lodi.



Figure 8: Miles Winery is across Seneca Lake from Lodi Point.

name and the number in you party Fair winds,
for Friday and Saturday. This is
just to get an idea for
reservations.

- Jim
o o o o o

From the Administrative Officer

By Shane Alsworth, S

MV The Office

Social Events



We had a wonderful social event on Friday, April 24th, at the Tin Barn Brewing in Dundee,

New York. Our Education Officer, Katie Alley, as well as Brock Sgrecci and Spencer Beaver, shared an engaging presentation about their experience successfully completing the 100-Ton Master’s Captain Course at Maritime Professional Training in Fort Lauderdale, Florida. The three presenters are fellow crew members aboard the charter boat, *True Love*, out of Seneca Harbor in Watkins Glen.

They described their motivations for pursuing the US Coast Guard Captain’s license as well as background on the intense two-week preparatory course they attended together. All three passed the education requirements, and with the completion of some remaining paperwork, will soon officially be USCG Captains! Katie acknowledged the excellent foundation that the America’s Boating Club courses provided in helping her achieve this milestone.

In attendance were many ABC-FLX members, guests and several prospective new members. We enjoyed craft beer, wine, tacos, finger food, entertaining stories and great boater fellowship.

Our next social event is in the final planning stage. Mark Wittig will be presenting on his decades

of experience helping boaters with repair projects as well as boat production and restoration. He is a former white water rafting guide and aquatic environment instructor. He has a deep knowledge of the boating industry and plenty of interesting stories to share. Please be on the lookout for an upcoming announcement with the date, time and location for the next event. We hope to see you there!

If you have ideas or suggestions on venues you would like to experience or speakers you would like to hear from, please email me at ao@abc-flx.org or call me at (607) 973- 2665. I look forward to hearing from you!

- Shane

ao@abc-flx.org



Figure 9: Attendees listen to the story of our newest captains.



Figure 10: Captains Spencer, Brock, Katie and Terry.

From the Education Director

By Katie Alley, JN

SV Tomfoolery

Uncertainties, Projects, and Activities



It's May 1st. My roommates (long term boyfriend and a good friend) and I have to decide

whether or not we're resigning our lease by the 10th. We still don't know what we're doing. Sigh.

The lake is currently too high to launch our boats at Watkins Glen Village Marina. It's been a rainy and dreary week, so who knows when we'll be able to get our boats floating. Sigh.

At least I can say I've spent the last couple of months productively, getting to know my C&C 25 and hopefully making myself a more knowledgeable boat owner and operator.

One of the first things I wanted to upgrade on my boat were the cushions. They were likely original, with the signature 80s beige and stripes pattern. Not only were they *ugly*, they smelled musty. The zippers on the fabric no longer even opened, so I had to cut the fabric off of the foam, on which I discovered years of water stains, mildew, and mold. Yeah, those have been rehomed to our local landfill.

I've been slowly but surely working on cutting and sewing 12 new cushions to completely upgrade my interior. I spent about

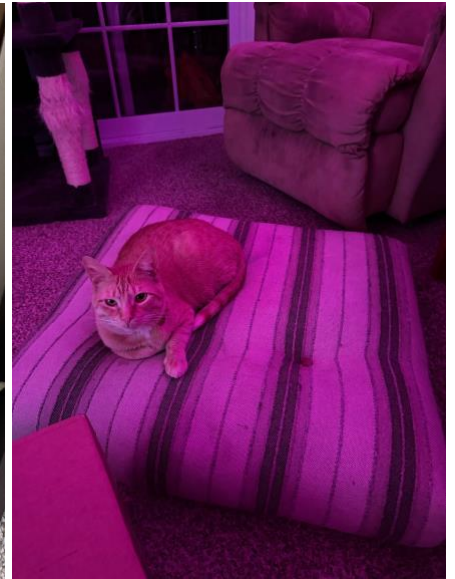


Figure 11: Hobie and Habanero seemed to think the original cushions were just fine.



Figure 12: Habanero "helping."

\$1300 at Sailrite for new foam and Outdura fabric that came in five comically large boxes. Luckily, borrowing the Sailrite sewing machine from my dad is free. One cushion came out a bit wonky, and I sewed the zipper cover on the wrong end, so I got mad and have fallen into a cycle

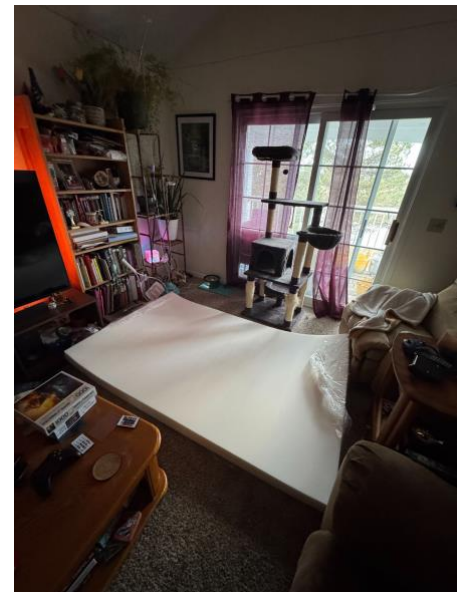


Figure 13: My 80"x48" piece of foam expanding in the living room. I have to laugh at the ridiculousness of doing this in an apartment.

of procrastination. I need to get back to it.

But before I can put the finished products on the boat, I think I need to acknowledge the leaks I have. I know the lake stays out and the bilge pump works. It's the topside that's the problem. I took

my shrink wrap cover off on April 19th. It was full of holes after taking a beating from the winter wind. My mast is currently down, so the rigging is very much cluttering the deck. I couldn't stand the restricted head room from the cover in combination with that. So off the cover went and in the cabin the rain came! I believe my toerail, stanchions, and forward hatch are to blame. Sigh.



Figure 14: Without her winter coat!

I was absolutely thrilled to discover my mast wiring is functional and my standing rigging is in good condition, minus the topping lift wire. Once I replace the steaming light and the topping lift, I *should* be able to step the mast and think about launching!

I also have plans to paint, buff, wax, and rename *Lanikai*. She deserves a little makeover to look her best as we start a new chapter together. The boatyard feels a bit like a metaphorical purgatory, so I'll be happy to get past the pearly gates and solve the rest of her problems while she's floating.



Figure 15: I got VC-17 paint dust in my eyes from sanding.

Activity has been ramping up for ABC-FLX as well. *Instructor Development* with Charlie Fausold wrapped up in early March and I have submitted three new candidates to become "officially recognized" instructors by National - Jim Morris, Scott Williams, and Kris West. I'm so glad that they completed the course and are committed to boating education in our area!

Two other classes, *Marine Navigation*, taught by Charlie Fausold and Ray Margeson, and *Marine Electrical Systems*, taught by Jim Morris and John Chesebrough, are wrapping up in early May. I am completing *Marine Electrical Systems*, and while it certainly was not my favorite class, I feel far less clueless looking at the wire spaghetti that came with *Lanikai*.



Figure 16: Jim Morris showing us around a multimeter.

From there, we'll be kicking off the boating season with the *Sail* class, taught by Jim McGinnis and Tom Alley. This class will continue through early July, with classroom sessions at the Watkins Glen Yacht Club and on-the-water session at Watkins Glen Village Marina. It should be a fun one!

Stay tuned for updates about our Summer Rendezvous, July 31-August 2. This one is going to be a bit different, with another seminar offered!

Also watch your email for invites to future social events. I was able to speak at our last gathering on April 24th, along with my *S/V True Love* crewmates Spencer Beaver and Brock Sgrecci, about our experience earning our USCG captain's licenses. It was great to see everyone again and I'm even more excited to see you back out on the water! Whenever we get past the uncertainties of water levels and projects tying us to boatyard purgatory.

- Katie
seo@abc-flx.org

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Secretary's Sidenotes

By Glenda Gephart

Other People's Boats!

Being a Statistic



Several years ago, I became a statistic by the joining the nearly 19 million Americans who pick up a paddle to enjoy their time

on the nation's waterways. For me, it's a kayak on a Finger Lake or two, on a pond near Lake Ontario or in a bay in southwest Florida.

I'm no expert, and my hours on the water are miniscule compared to friends', but we all know that paddle sports are fun and relaxing and a great way to appreciate the outdoors.

Taking the "Paddle Smart" class offered in August 2018 was my initiation into America's Boating Club. Not only did I learn kayaking essentials, I became aware of our chapter's dedication to boating safety and education. I joined ABC-FLX soon after completing the kayaking course.

The classes, hosted by the Watkins Glen Yacht Club in their clubhouse, covered terminology, how to select a paddle craft, minimal and optional equipment, safety practices, considerations for day-tripping and touring and so much more. We all learned a lot!

Fun fact: During that same summer, ABC-FLX members joined a crew from America's Boating Channel to produce a video about safe paddling. The five-minute video, "Safety Tips for Paddle Sports," can be seen on YouTube at

<https://www.youtube.com/watch?v=tLNfnbin7ag>.

The video was funded by a grant from the Sport Fish Restoration and Boating Trust Fund, administered by the U.S. Coast Guard.

It is worth a watch, not only for the safety tips, but also to see some very familiar faces making their acting debuts.

Back to the statistics. According to the website WifiTalents.com, which offers a huge variety of data, 18.6 million Americans participated in paddling activities in 2022. The global kayak market size was valued at approximately \$181.2 million that year, with the United States dominating with a share of around 40 percent.

Recycled plastic was used in 5 percent of all kayaks produced in 2023. Manufacturers introduce more than 200 new kayak models annually in North America. The average weight of a recreational kayak is 45-55 pounds. The



Figure 17: Kayaking the shallow waters of the south end of Owasco Lake.

inflatable kayak segment is expected to register the fastest compound annual growth rate of 4.2 percent through 2030.

And then there are these numbers: Paddle craft accounted for 15 percent of all recreational boating fatalities in 2022. Victims not wearing a life jacket accounted for 75 percent of kayak-related fatalities. Eighty percent of kayaking accidents occur on calm or slow-moving water. Alcohol use is a primary factor in 10 percent of fatal kayaking incidents. Improper loading was the cause of 12 percent of non-fatal kayak accidents.

Lastly: Educational safety courses reduce the risk of accidents by 25 percent among new boaters. Do you think it's time for another "Paddle Smart" course offered by ABC-FLX? Let us know!

- Glenda
secretary@abc-flx.org

ABC-FLX News

Happy Birthday!

Happy birthday to our members!

May

Ray Margeson
Denis Kingsley
Jacqueline Alsworth
Don Kloeber
Scott Williams
Phil Cherry

June

Dennis Daniels
Bill Phoenix
Tom Taylor
Linda Sampson

District 6 Change of Watch

Congratulations to the new Bridge for District 6! The following people pledged to serve our district at the D/6 Spring Conference in Binghamton on April 18th:

- Michael Acciai, Commander
- Emy Casper, Executive Officer
- Cathie Johnstone, Administrative Officer
- Ira Goldman, Education Officer
- Clark Godshall, Assistant Education Officer

- Chris Foster, Secretary
- Peg Acciai, Assistant Secretary
- Bridgett Arieno, Treasurer
- Nancy Bieber, Assistant Treasurer

Got News?

If you have news to share that you think would be of use to your fellow boaters, please submit it to your friendly newsletter editor so that it can be included!

editor@abc-flx.org



Upcoming Classes & Seminars

Where Do I Start?

To work through the progression of Boating Classes and the progression of Boating skills development we have organized the classes and skills on the Long-Term Schedule. If you have any questions or want a class offered sooner, let Katie, our Education Officer, know.

See also: <https://usps.org/sss-where-do-i-start>

Sail

This course lets you learn about basic sailboat designs and nomenclature, rigging, and safety from experienced sailors. Then tackle the physical aspects of all forces and techniques, sail applications, marlinespike, helmsmanship, and handling of difficult conditions.

This course also features the Finger Lakes Chapter trademark on-the-water, hands-on instruction.

Prerequisites: None

When: Starts May 6th

Where:

- Classroom: Watkins Glen Yacht Club (19:00)
- On-the-Water: Watkins Glen Village Marina (18:30)

Instructor: Jim McGinnis, Tom Alley, and more!

Cost: \$120 (member)

Anchoring Seminar

Join us “on location” during the 2026 Summer Boating Weekend for a hands-on, on-the-water seminar on anchoring your boat.

Prerequisites: None

When: Aug 1st

Instructors: Jim McGinnis, Tom Alley

Cost: Free (no seminar credit awarded) or \$25 (for seminar credit from National)

Engine Maintenance

Another popular elective that, like its electrical cousin, demystifies the mechanical beast living in the bowels of your boat. Like many of our other courses, this one also has a hands-on component.

Prerequisites: None

When: Late-Summer/Early-Fall 2026 (Q3)

Instructors: TBA

Cost: TBA

How to Register

If you have questions about any of these courses, or better yet, if you wish to sign up, please contact: Finger Lakes Chapter Education Director, Katie Alley:

seo@abc-flx.org

Or you can go to the national web site (www.usps.org) and register under the “Find A Boating Class” tab on the member home page.

Looking for Something?

ABC-FLX would be happy to hear your requests and ideas. Feel free to contact me, Katie Alley, at

SEO@abc-flx.org.

Available subjects for instruction can be found on the chapter web site:

www.abc-flx.org

Quote of the Day:

Nothing spoils a good story like the arrival of an eyewitness.

- Mark Twain

Get Ready for the Season: Schedule Your Vessel Safety Check

By Ray Margeson

SV Hattie L

As boating season approaches, America’s Boating Club Finger Lakes Chapter is once again offering **free Vessel Safety Checks (VSCs)** to help members and local boaters start the year with confidence. These courtesy inspections—performed by certified examiners—ensure your vessel meets federal and state safety requirements and is equipped for a safe day on the water.

Why a Vessel Safety Check Matters

- **It’s completely free and non-regulatory.** Examiners are there to help, not to enforce laws or issue penalties.
- **You’ll know your boat is properly equipped.** From life jackets and fire extinguishers

to navigation lights and documentation, the exam covers the essentials.

- **It reduces the chance of breakdowns or emergencies.** A quick check now can prevent a stressful situation later.
- **You may earn a VSC decal.** Passing vessels receive a 2026 safety sticker recognized by marine patrols, often resulting in fewer on-water stops.
- **It promotes safer boating for everyone.** A well-prepared skipper sets the tone for a safer community on the water.

How to Get Yours

America’s Boating Club examiners are available throughout the season and can meet you at your marina, launch



ramp, or home dock. Whether you’re a new boater or a seasoned captain, a Vessel Safety Check is one of the simplest ways to ensure you’re ready for the adventures ahead.

Just email vsc@abcflx.org to request your safety check. We have three qualified inspectors to do the assessment.

- Ray

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A Letter from a 100-Year-Old Wooden Schooner

by *SV True Love*

Watkins Glen, NY

My name is *True Love*. A naval architect named John Alden drew my mother's lines and my mother-ship was built for Mr. Alden. He built ten boats in the *Malabar* series from 1920 to 1930. He had *Mom (Malabar VII)* built to race in the Newport to Bermuda race in 1926. *Mom* won that race in 1926. Mr. Alden didn't think she could, but she did. *Mom* had six offspring, of which I was one. My sisterships were named *Angelica, Teal, Fearless, Sea dream, and Moby Dick*. I do not believe any of my sisters are still with us. I was originally named *Verona II*. I have original papers that describe exactly how I was built to the standards required by Mr. Alden.

I was built at the Pendleton Brothers boat yard in Wiscasset, Maine. I've had quite the lucky life for a wooden schooner. Elmer Bliss was my first owner who named me *Verona II* because he had earlier owned a boat, *Verona*. In my early, youthful days I was a privately owned family boat and was sailed for their own enjoyment. Elmer was a shoe manufacturer in Boston and created a company called *Regal Shoes*. He was very successful and took good care of me. Some years later, when I was renamed *True Love*, I was owned by Charles Peake in Connecticut, who was an executive with the *Vicks Corporation*. In those days I sailed largely around Cape Cod and in Buzzards bay. In 1956 I was featured in a Hollywood movie *High Society!* I think it was

because I was named *True Love* and the movie was a musical romantic comedy. I actually had Bing Crosby singing to Grace Kelly in my cockpit. I was very honored to be the sailboat in this movie and it kind of made me a little famous too.

Sadly, on September 12th, 1960, there was a Hurricane named *Donna*, which I was caught in, at Stamford, Connecticut. A tree fell onto my mooring line, and severed it which caused me to wash into a rock jetty and my stern was damaged so badly that I sank in the mooring field. It was a bad day for me. I thought I was a goner. Thankfully, I was raised up and repaired, and by 1962 I was back sailing again. A new chapter was in store for me.

By 1965 a family named the Beer's owned me and made me a charter boat! A new life I'd never experienced before. I was sailing people every day and I was very popular. I never knew how much joy I could bring to so many people. I was sailing "honeymooners" all the time. This life went on for over 35 years.



Figure 18: True Love under sail on Seneca Lake.

I began to tire over the years and when I was 75 years old I needed some rebuild. I was brought to Newport, RI and work was done on my planking and other parts. Back again to St. Thomas for a few years and I was for sale again in 2006. Then, in 2008, a nice couple, Josh and Lisa Navone, found me in St. Thomas, purchased me, and sailed me back to a place I'd never been before: New York State and Seneca Lake, in the Finger Lakes. This turned out to be another lucky break for me. The Navone's took loving care of me, and in 2009 I was sailing people every day again. I do hate the winters here because I'm lonely and cold from November to April. It's nothing like St. Thomas! I spend most winters tied up in the Village



Figure 19: Part of True Love's loving crew. (L to R) Captain Terry Stewart, Captain Katie Alley, Captain Bill Holgate, and Captain Brock Sgrecci.

Marina, where the winter winds chill me to the bone and sometimes minks crawl on me.

Well, now I've been living on Seneca Lake since 2009. Once, in October of 2019, Josh and Lisa had a dedicated captain and crew who helped them sail me to the Chesapeake Bay Schooner Race where I won my class! Then I went on to Key West for the winter. Something unfortunate happened when I was in Key West. The Covid outbreak happened. I was basically

stranded in Key West and could not get home until April, and then the Erie Canal was not open so I had to stay in Albany on the Hudson river at the Scarano Brothers boatyard. I could do no sailing during the summer of 2020. I was a little unsure of what was happening to me.

At this time, Josh and Lisa Navone sold me to Terry Stewart.

Thankfully, enough people care about me to keep me thriving. My new owner and his dedicated people brought me down the Erie canal once more (a great motor ride, by the way) and I was back bringing joy to my passengers once again. From 2021 until now I have done nearly 1,700 two-hour sails with over 25,000 guests aboard.

My captains and crew love me and I love them back. We have a wonderful time each year from May to October.

I hear I am having a 100 year birthday celebration on September 25th and 26th. It will be in The Village Marina and headquartered at Lucky Hare Brewing in the marina. They are planning a classic wooden boat display, sea shanty and Erie Canal music, a regatta, and the movie I am in, *High Society*, being shown in the park! I also have former owners from far away coming to the party too! This could be fantastic and something every old schooner would dream of.

Thank you all for keeping me healthy enough to keep sailing every day, and I am looking forward to a great season and a memorable 100 year birthday event!

- True Love

o o o o o

All USPS members and friends are invited to join in the festivities in Watkins Glen, NY on September 25th and 26th as we celebrate the centennial of the classic John Alden schooner known as True Love!

More information can be found about the True Love on her web site at <https://SailTrueLove.com>.

The Last Word

By Tom Alley, SN-ON

SV Tomfoolery

Time to Execute



With warmer days beginning to outnumber the colder ones, it's time to stop planning and to begin executing

those carefully curated intentions that have accumulated over the winter months. In my case, I've been hauling tools, pieces, and parts for the new autopilot that will be installed aboard *Tomfoolery* this spring.

The old autopilot, a unit I installed 30 years ago shortly after acquiring *Tomfoolery*, is being held together mostly through sheer stubbornness and wishful thinking. (...with help from some glue, bailing wire, and duct tape!) Three decades of exposure to UV-rich sunshine has taken its toll on the plastic components. It was time.

The new autopilot will go below deck, where it won't suffer as much from exposure to the elements. The price for this sanctuary, of course, will be

numerous sessions involving rather strenuous "boat yoga" as I try to fit myself, some tools, and the parts that need to be installed into spaces never designed for human occupancy.

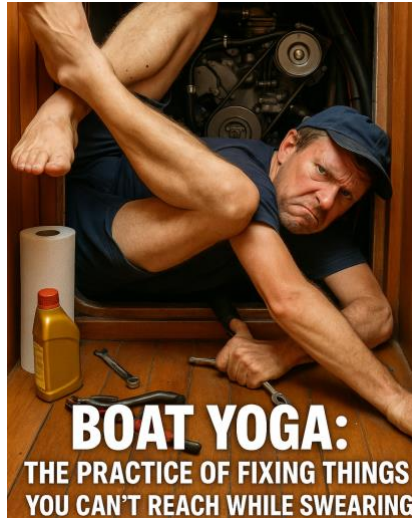


Figure 20: Image from the Kingston Yacht Club Facebook page.

Web Site Updates

Hopefully you received an email recently from our Chapter Commander, Kris West, asking for input on our new web site. I'm pleased to report that work is proceeding on budget and slightly ahead of schedule. We are rapidly approaching a milestone where we will "freeze" all feature requests in

preparation for the big jump from one web server to another. If you have any input for the development team, please send Kris or Glenda your suggestions. Soon!

Special kudos to Glenda Gephart who has been representing ABC-FLX on the committee doing the migration and upgrade. Also, a big welcome to Daniel Villanti who has volunteered to assist Glenda in this endeavor.

Heightened Awareness

If you have been paying attention, you know that water levels in Seneca Lake are up. To be precise, they're up to where the the docks in the Village Marina are just visible. This means launches are being delayed until water levels drop to the point where fenders will be effective.

Until then, I'll see you in the boatyard!

- Your Editor, Tom

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As always, send your thoughts about this newsletter to:

editor@abc-flx.org

Long-Term Class Schedule

Seneca Education Department

BOC Level	Title	2026	2027	2028	2029	2030	2031	
Inland Navigator	Classes	ABC	X	X	X	X	X	
		Boat Handling		X		X		X
		Engine Maintenance	X		X			
		Marine Electrical Systems	U					
	Seminars	Using A Chart						
		VHF/DSC Radios						
		Using GPS			X			
		Basic Weather and Forecasting		X				
	Skills	Basic Powerboat Handling			X			
		Fire Extinguishers						

BOC Level	Title	2026	2027	2028	2029	2030	2031
Coastal Navigator	Classes	Marine Navigation	C				X
		Marine Communication Systems		X			
		Weather				X	
	Seminars	Tides & Currents					X
		Rules of the Road				X	
		Anchoring					
		Mariner's Compass	X		X		
	Skills	Coastal Nav			X		
		Pyrotechnics		X			

Table Key

X = Planned

U = Underway

C = Completed

BOC Level		Title	2026	2027	2028	2029	2030	2031
Advanced Coastal Navigator	Classes	Advanced Marine Navigation	X		X		X	
		Cruise Planning				X		
		Radar						
		Emergencies Onboard				X		
	U							
	Skills	Advanced Coastal Nav				X		
First Aid		<i>Contact American Heart Assoc. or Red Cross</i>						

BOC Level		Title	2026	2027	2028	2029	2030	2031
Offshore Navigator	C	Offshore Navigation		X			X	
	Sem.	Computer Weather Forecasting	<i>Offered online.</i>					
		Thunderstorms / Severe Weather	<i>Offered online.</i>					
	Skills	Offshore Navigation				X		
		CPR/AED	<i>Contact American Heart Assoc. or Red Cross.</i>					
Endorsments		Sail	X		X		X	
		Canadian Regulations						
		PaddleSmart	X		X			
Other		Instructor Development	C		X			
		Instructor Recertification	X			X		
		Operations Training	<i>Offered online.</i>					
		Celestial Navigation			X			X

Table Key

X = Planned

U = Underway

C = Completed

Calendar of Events

May 2026

- 01 Finger Lakes *Drum* May issue publication date.
- 01 *The Deep 6* spring issue publication date. (D/6)
- 05 Sail class begins, *WGYC* (1830)
- 12 Bridge Meeting (1900)
- 16-22 Safe Boating Week (National)

June 2026

- 09 Bridge Meeting (1900)
- 12-14 FLYC Commodore's Cup Race/Cruise to Geneva Weekend (FLYC)
- 19 Deadline for *Drum* articles

July 2026

- 01 Finger Lakes *Drum* July issue publication date.
- 14 Bridge Meeting (1900).
- 17 Deadline for *The Deep 6* articles. (D/6)
- 31-8/2 ABC-FLX Boater's Weekend. *Details TBA.*

August 2026

- 01 *The Deep 6* summer issue publication date. (D/6)
- 7/31-8/2 ABC-FLX Boater's Weekend. *Details TBA.*
- 11 Bridge Meeting (1900)
- 21 Deadline for *Drum* articles

September 2026

- 01 Finger Lakes *Drum* September issue publication date.
- 08 Bridge Meeting (1900)
- 12 Seneca Lake Barge Race, *Seneca Yacht Club, Geneva, NY.*
- 20-27 Fall Governing Board, *Cary, NC* (National)

October 2026

- 13 Bridge Meeting (1900)
- 16 Deadline for *The Deep 6* articles (D/6)
- 23 Deadline for *Drum* articles
- TBA District 6 Fall Council & Conference (D/6)

November 2026

- 01 Finger Lakes *Drum* November issue publication date.
- 01 *The Deep 6* fall issue publication date. (D/6)
- 10 Bridge Meeting (1900)

December 2026

- 08 Bridge Meeting (1900)
- 18 Deadline for *Drum* articles

January 2027

- 01 Finger Lakes *Drum* January issue publication date
- 12 Bridge Meeting (1900)
- 15 Deadline for D/6 *The Deep 6* articles. (D/6)
- TBA Finger Lakes Change of Watch

February 2027

- 01 *The Deep 6* winter issue publication date. (D/6)
- 06-13 USPS Annual Meeting, *Tampa, FL* (National)
- 09 Bridge Meeting (1900)
- 19 Deadline for *Drum* articles

March 2027

- 01 Finger Lakes *Drum* March issue publication date.
- 09 Bridge Meeting (1900)

April 2027

- 13 Bridge Meeting (1900)
- 16 Deadline for *The Deep 6* articles (D/6)
- 23 Deadline for *Drum* articles
- TBA District 6 Spring Council & Conference (D/6)

Calendars are "living documents." For the latest information on squadron activities, please check our web site or our Facebook page:

<http://www.abc-flx.org>

<http://facebook.com/SenecaPowerSquadron>